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**F/YR20/0427/F**

**Applicant: Ms Wendy Otter  
Fenland District Council**

**Agent :**

**Land North Of Manea Railway Station Access Via, Fodder Fen Road, Manea,**

**Formation of a car park (to serve Manea Train Station) and the formation of an access and 2m (max) wide footpath link**

**Officer recommendation: Grant**

**Reason for Committee: Fenland District Council are the applicants**

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## **1 EXECUTIVE SUMMARY**

- 1.1 The application seeks full planning permission for a 112 space car park in association with Manea railway station, including the formation of a new access off Fodder Fen Road and 2m (max) wide footpath link to the station. Surrounding the car park is a 6m buffer from the boundary hedging and a 30m buffer from the overgrown area to the north, required for ecology purposes.
- 1.2 Policies LP1, LP2 and LP15 of the Fenland Local Plan seek to encourage delivery of a more sustainable transport network. The Fenland Infrastructure Delivery Plan strives to improve rail service usage and proposes a car park near Manea station to support this aim.
- 1.3 The application site is considered to relate more to the countryside than the built settlement and as such it is important that this character is retained as much as possible to limit the impact. The formalisation of the access and creation of a 2m (max) wide footway would have an urbanising impact, however this would be outweighed by the public benefit of the scheme and need to ensure that well designed, safe and convenient access for all is achieved. The retention and enhancement of the boundary vegetation will mitigate the impact of car park from the streetscene.
- 1.4 There are a number of dwellings surrounding the site and there is potential that external lighting could have an adverse impact on residential amenity. It is therefore felt necessary to impose a condition in relation to lighting, to ensure adequate mitigation is provided.
- 1.5 Community safety concerns have been raised; security measures such as CCTV, lighting (as mentioned above) along with the management and maintenance of the site have not been confirmed, hence it is necessary to impose a condition in this regard to ensure suitable measures are put in place.
- 1.6 The Local Highway Authority have no objections to the proposal subject to conditions ensuring the provision of the access and footpath. Full details of the footpath design have not been provided and there is potential for this to have an impact on surrounding trees, hence a condition will be imposed to ensure adequate mitigation is secured.

- 1.7 The proposed works will result in the loss of a small area of foraging/commuting habitat, however impacts are not anticipated to be significant. Conditions will be imposed in relation to the retention of the existing boundary vegetation and buffer zone in addition to the recommended ecology enhancement/compensatory measures.
- 1.8 The principle of this development is supported. Subject to the retention and enhancement of the boundary vegetation the proposal is not considered to have a significant detrimental impact on the character of the area. There are no further requirements in relation to archaeology and it is considered that suitable drainage, landscaping, security, lighting and ecology mitigation can be achieved. It is therefore recommended to grant the application

## 2 SITE DESCRIPTION

- 2.1 The application site is a roughly triangular shaped piece of land located to the north-east of the Manea railway station, on the western side of Fodder Fen Road. The site is bounded by trees and vegetation and is partially overgrown, there are electricity lines which run through the site and an informal gated access off Fodder Fen Road. The site falls within Flood Zone 3, the highest risk of flooding.

## 3 PROPOSAL

- 3.1 The application seeks full planning permission for a 112 space car park in association with Manea railway station, including the formation of a new access off Fodder Fen Road and 2m wide footpath link to the station. Surrounding the car park is a 6m buffer from the boundary hedging and a 30m buffer from the overgrown area to the north, required for ecology purposes.
- 3.2 Full plans and associated documents for this application can be found at:

<https://www.fenland.gov.uk/publicaccess/applicationDetails.do?activeTab=documents&keyVal=Q8XIE8HE03000>

## 4 SITE PLANNING HISTORY

F/YR14/0113/F	Erection of 3no dwellings comprising of 1 x 2-storey 4-bed with detached double garage/workshop/store, 1 x 2-storey 4-bed with attached garage with store above and 1 x 4-bed with attached double garage	Refused 01/07/2014  Dismissed on Appeal 09/01/2015
F/YR12/0936/F	Erection of 10 x 2-storey dwellings comprising of: 7 x 3-bed dwellings with associated parking, 1 x 3 -bed dwelling with integral garage, 1 x 4-bed dwelling with detached double garage and store, 1 x 4-bed dwelling with an attached double garage, a biomass cabin and the formation of 2 x accesses, a pond and 18 carparking spaces	Refused 09/07/2013

F/YR02/1455/F	Erection of 2 workplace homes	Refused 25/6/2003
F/YR02/0782/O	Erection of 2 dwellings	Refused 21/8/2002
F/YR01/0121/O	Erection of a 4-bed detached house	Refused 16/3/2001
F/YR01/0786/O	Erection of a 4-bed house	Refused 14/9/2001

## **5 CONSULTATIONS**

### **5.1 Parish Council (16/6/2020)**

*Members approved the application, however with comments:*

- *Pedestrian routes should be fully accessible and DDA compliant.*
- *Taxi pick up and drop off*
- *Cycle park and separation*
- *Turning provision for replacement bus*
- *Bus stop on Fodder Fen Road for future shuttle bus drop off/pick up*
- *Access control*
- *All services supplies should be fully sustainable*
- *Ablution facilities*
- *Rainwater run off (SUDS)*

*Separate entry/egress to be considered for future increase in traffic and to reduce queuing on the highway when the level crossing is closed.*

### **5.2 Parish Council (1/9/2020)**

*Members received the revised proposals and Ecology Report. Members are still concerned that their previous comments are still not included in the revised proposals.*

### **5.3 Designing Out Crime Team (1/6/2020)**

*Thank you for the opportunity to comment on this application. I have viewed the documents in relation to crime, disorder and the fear of crime and have completed a crime and incident search for Manea covering the last 12 months. I would consider this to be an area of low vulnerability to the risk of crime.*

*I do have two questions: -  
Will the car park be closed over-night after the last trains?  
What lighting is being proposed?*

*I have no further comment or objections at this time.*

### **5.4 Designing Out Crime Team (20/8/2020)**

*Thank you for the opportunity to comment on this application and revised proposals. I have viewed the documents in relation to crime, disorder and the fear of crime and note the responses in the supporting statement dated 16/06/2020 relating to lighting and operation of the car park.*

*I have no further comment at this time.*

## **5.5 Environmental Health (FDC)**

*It would also be reasonable to expect the applicant to provide a full light impact assessment to demonstrate that proposed lighting is in accordance with the Institution of Lighting Professionals Guidance Notes for the Reduction of Obtrusive Light GN01:2011. The accompanying report shall include the lighting specifications, proposed positioning, as well as an Iso Contour to clearly demonstrate what light overspill levels are to be expected from the site, and ensure that they do not adversely impact on existing nearby residential properties.*

*This can be imposed as a condition.*

## **5.6 Environmental Health (FDC) (3/9/2020)**

*Having reviewed the revised information submitted, I can confirm the Environmental Health team have no further comment to make on this application.*

## **5.7 Lead Local Flood Authority**

*Thank you for your consultation which we received on 1st June 2020.*

*We have reviewed the following documents:*

*Flood Risk Assessment and Drainage Strategy, Create Consulting Engineers Ltd, Ref: TT/CC/P19-1853/01, Dated: March 2020*

*Based on these, as Lead Local Flood Authority (LLFA) we have no objection in principle to the proposed development.*

*The above documents demonstrate that surface water from the proposed development can be managed through the use of permeable paving over the extent of the car parking and access areas. The permeable paving will provide storage before discharge to the watercourse to the east of the site at a rate of 1.0l/s. It will be explored whether the watercourse can be lowered slightly to allow a gravity connection, however if this is not feasible, a pumped outfall will be required.*

*Water quality has been adequately addressed when assessed against the Simple Index Approach outlined in the CIRIA SuDS Manual.*

*We request the following condition is imposed:*

### *Condition*

*No above ground works shall commence until a surface water drainage scheme for the site, based on sustainable drainage principles, has been submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in full accordance with the approved details prior to occupation of the first dwelling.*

*The scheme shall be based upon the principles within the agreed Flood Risk Assessment and Drainage Strategy prepared by Create Consulting Engineers Ltd (ref: TT/CC/P19-1853/01) dated March 2020 and shall also include:*

*a) Full results of the proposed drainage system modelling in the 100%, 3.3% and 1% Annual Exceedance Probability storm events (as well as 1% AEP plus climate change), inclusive of all collection, conveyance, storage, flow control and disposal*

elements, together with an assessment of system performance. This should include pump failure modelling representing full pump failure and 50% storage; b) Detailed drawings of the entire proposed surface water drainage system, including levels, gradients, dimensions and pipe reference numbers; c) Full details of the proposed attenuation and flow control measures; d) Details of overland flood flow routes in the event of system exceedance, with demonstration that such flows can be appropriately managed on site without increasing flood risk to occupants;

*The drainage scheme must adhere to the hierarchy of drainage options as outlined in the NPPF PPG*

*Reason To ensure that the proposed development can be adequately drained and to ensure that there is no increased flood risk on or off site resulting from the proposed development*

#### *Informatives*

*IDB Consent Part or all of your proposed development area falls within the Middle Level Commissioners (MLC) catchment and/or that of Manea and Welney IDB whose consents are managed by the MLC. All increased discharges proposed to enter watercourses directly or indirectly or any works affecting watercourses or access to or along them for maintenance if the site is within the Board's district will require MLC/IDB consent. It is therefore recommended that you contact the IDB/MLC to discuss their requirements. Further information is available at: <https://middlelevel.gov.uk/>*

*Pollution Control Surface water and groundwater bodies are highly vulnerable to pollution and the impact of construction activities. It is essential that the risk of pollution (particularly during the construction phase) is considered and mitigated appropriately. It is important to remember that flow within the watercourse is likely to vary by season and it could be dry at certain times throughout the year. Dry watercourses should not be overlooked as these watercourses may flow or even flood following heavy rainfall.*

#### **5.8 Cambridgeshire County Council Highways (11/6/2020)**

*To simplify the conditions and to avoid pre-commencement conditions, can you request the applicant annotates the access with the following notations;*

- Highway access crossover to be constructed to CCC Highways Construction Specification.*
- Private access road to be sealed and drained away from the public highway.*

*Defer for amended plans or re-consult for highway conditions.*

#### **5.9 Cambridgeshire County Council Highways (3/7/2020)**

*I have no highway objections subject to the following conditions recommendations;*

*1.) Prior to first use, a 2.0m wide footway will be constructed along Fodder Fen Road in accordance submitted plan TP54-11 Rev D.*

*Reason: In the interest of satisfactory pedestrian access.*

*2.) Prior to first use, the vehicular access will be constructed in accordance submitted plan TP54-11 Rev D*

*Reason: In the interest of satisfactory vehicular access.*

## *Informative*

1.) *This development involves work to the public highway that will require the approval of the County Council as Highway Authority. It is an OFFENCE to carry out any works within the public highway, which includes a public right of way, without the permission of the Highway Authority. Please note that it is the applicants responsibility to ensure that, in addition to planning permission, any necessary consents or approvals under the Highways Act 1980 and the New Roads and Street Works Act 1991 are also obtained from the County Council. Public Utility apparatus may be affected by this proposal. Contact the appropriate utility service to reach agreement on any necessary alterations, the cost of which must be borne by the applicant.*

2.) *The applicant should note that the nature of the highway works proposed will necessitate the completion of a Short Form 278 Highway Works Agreement between the developer and the LHA prior to commencement.*

### **5.10 Middle Level Commissioners IDB**

No comments have been made to the LPA in relation to the application.

### **5.11 Arboricultural Officer (FDC)**

*If the trees are in third party ownership, the developer must take the necessary measures to protect those trees from damage. Whilst Common Law allows for the cutting of both branches and roots that cross the property boundary, the work must not make the trees hazardous e.g. by severing roots leading to instability or decline in their health.*

*There is an existing grass verge at road level sloping down and away from the road and there would appear to be potential to install a minimal dig footpath using a cellular confinement system with porous tarmac as a final wearing surface.*

*Another option would be to approach the tree owners with a view to removing the trees constraining the development and replanting following completion of the construction phase.*

### **5.12 Local Residents/Interested Parties**

One objection has been received (from a resident of Station Road, Manea) in relation to the following:

- Out of scale with the village
- Improved railway infrastructure needed
- Potential for anti-social behaviour and issues that come with this

Four representations have been received (from residents of Wisbech Road, Fodder Fen Road and Westfield Road, Manea) in relation to the following:

- Concerns regarding encroachment of footpath on their land
- Access to their land/devaluation of property
- Option to utilise land for access to platform
- Why is the railway land the other side of the road not being used
- Scale of car park large for those that use it
- Queries regarding the speed limit, hours of access, maintenance, lighting and fees

- Request for disabled spaces located at the nearest exit point, children's/family spaces and those for larger vehicles
- whether there is a shorter route possible to minimise the distance a person needs to travel to get to the platform
- Access width should be large enough for pedestrians, wheelchair & mobility scooter access.

Comments, where they relate to planning matters will be considered in the sections below.

- 5.13 It should be noted that only the proposal being put forward can be considered, though comments from the Parish Council have been passed to the applicant. Devaluation of property and rights of access are not planning matters.

## **6 STATUTORY DUTY**

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires a planning application to be determined in accordance with the Development Plan unless material planning considerations indicate otherwise. The Development Plan for the purposes of this application comprises the adopted Fenland Local Plan (2014).

## **7 POLICY FRAMEWORK**

### **National Planning Policy Framework (NPPF)**

### **National Planning Practice Guidance (NPPG)**

#### **National Design Guide 2019**

Context – C1

Identity – I1

Movement – M3

Nature – N3

#### **Fenland Local Plan 2014**

LP1 – A Presumption in Favour of Sustainable Development

LP2 – Facilitating Health and Wellbeing of Fenland Residents

LP3 – Spatial Strategy, the Settlement Hierarchy and the Countryside

LP13 – Supporting and Managing the Impact of a Growing District

LP14 – Responding to Climate Change and Managing the Risk of Flooding in Fenland

LP15 – Facilitating the Creation of a More Sustainable Transport Network in Fenland

LP16 – Delivering and Protecting High Quality Environments across the District

LP17 – Community Safety

LP18 – The Historic Environment

LP19 – The Natural Environment

#### **Delivering and Protecting High Quality Environments SPD 2014**

DM2 – Natural Features and Landscaping Schemes

#### **Cambridgeshire Flood and Water SPD 2016**

#### **Fenland Rail Development Strategy 2011-31**

#### **Fenland Infrastructure Delivery Plan (IDP) 2016**

## 8 KEY ISSUES

- **Principle of Development**
- **Design considerations and visual amenity of area**
- **Residential Amenity**
- **Community safety**
- **Parking and Highways**
- **Flood Risk/Surface Water Drainage**
- **Ecology**
- **Archaeology**
- **Land ownership**

## 9 ASSESSMENT

### **Principle of Development**

- 9.1 The application site is located to the north of the railway line and does not therefore adjoin the developed footprint of the village, as such it would be classed as an 'elsewhere' location within the settlement hierarchy and Policy LP3 of the Fenland Local Plan 2014.
- 9.2 Development at such a location will be restricted to that which is demonstrably essential to the effective operation of local agriculture, horticulture, forestry, outdoor recreation, transport or utility services.
- 9.3 Policies LP1, LP2 and LP15 of the Fenland Local Plan seek to encourage delivery of a more sustainable transport network. The Fenland Infrastructure Delivery Plan strives to improve rail service usage and proposes a car park near Manea station to support this aim. Chapter 9 of the NPPF 2019 seeks to promote sustainable transport and encourage the use of public transport. As such the principal of this development is supported.

### **Design considerations and visual amenity of area**

- 9.4 The application site is considered to relate more to the countryside than the built settlement and as such it is important that this character is retained as much as possible to limit the impact. The site is bounded by trees and vegetation which it is proposed in the main to retain and enhance, a buffer also surrounds the car park, in the main for ecology purposes, however this also mitigates the impact of the development on the character of this rural location; it is proposed to condition these to ensure their retention. Full details of the materials to be used and proposed landscaping have not been provided, however these can be dealt with by way of a condition
- 9.5 The formalisation of the access and creation of a 2m wide footway would have an urbanising impact, however this would be outweighed by the public benefit of scheme and need to ensure that well designed, safe and convenient access for all is achieved.
- 9.6 There are a number of trees, including those on third party land that could be affected by the proposal. The Council's Arboricultural Officer has advised that there are design solutions to ensure that the trees, in particular those on third party land, would not be detrimentally impacted by the proposed works. Full details of the scheme are still to be agreed and this shall be informed by an appropriate tree survey to ensure adequate mitigation is incorporated.

### **Residential Amenity**



- 9.7 There are a number of dwellings in the vicinity of the site, 2 dwellings to the east on the opposite side of Fodder Fen Road, a dwelling to the north in association with a farm, this is located approximately 20m from the site but at least 50m from the area of the development due to the required buffer. To the south of the railway line is a workplace home estate and further dwellings on Station Road.
- 9.8 There is potential for noise and disturbance from the use of the car park and the encouragement for additional commuters to use the station, however given the scale of the development this is not considered to be significantly adverse and would be mitigated to some degree by the retention of the boundary trees and vegetation.
- 9.9 Details of external lighting have not been provided and there is potential that this could have an adverse impact on residential amenity, particularly if the lighting is in use early or late in the day. For this reason it is felt necessary to impose a condition in relation to lighting, to include a full light impact assessment and ensure a suitable scheme is provided which is fit for purpose but also would not have a significant detrimental impact on surrounding dwellings.

### **Community Safety**

- 9.10 Due to the need to retain the boundary vegetation and trees for ecology and visual amenity purposes there is limited natural surveillance of the site. Concerns have also been raised regarding the use of the site for anti-social behaviour.
- 9.11 Full details of lighting, security measures such as CCTV along with the management and maintenance have not been confirmed, hence it is necessary to impose a condition in this regard to ensure a safe environment is achieved.

### **Parking and Highways**

- 9.12 The development proposes the formation of an access off Fodder Fen Road and 2m (max) wide footpath to link the site with the railway station platform. Within the site are 112 parking spaces.
- 9.13 The Local Highways Authority have no objections to the proposal subject to conditions ensuring the provision of the access and footpath. Full details of the footpath design have not been provided and there is potential for this to have an impact on surrounding trees, hence a condition will be imposed to ensure adequate mitigation is secured.
- 9.14 The parking spaces are 2.5m x 5m which is considered to be adequate and there is at least 6m behind each space to enable these to be successfully entered/exited. The applicant has confirmed that it is the intention to incorporate disabled spaces within the scheme.

### **Flood Risk/Surface Water Drainage**

- 9.15 The application site falls within Flood Zone 3, the highest risk of flooding. The sequential test, which seeks to steer new development to areas at the lowest risk of flooding would be applicable to this development. The proposal is to provide car parking to serve Manea railway station and as such would need to be located in close proximity, there is no land nearby which is at lesser risk of flooding, hence the sequential test is considered to be passed.
- 9.16 The development is considered to be 'less vulnerable' within the flood risk vulnerability and flood zone compatibility table and as such is considered appropriate development, the exception test is therefore not required.

- 9.17 Flood risk mitigation measures are set out in Section 7 of submitted Flood Risk Assessment and incorporate such measures as flood warning information signs and the closure of the site if flooding is expected.
- 9.18 Surface water will be attenuated using SUDS; the Lead Local Flood Authority have no objections to the proposal subject to a condition, and advise that submitted documents demonstrate that surface water from the proposed development can be managed through the use of permeable paving over the extent of the car parking and access areas

### **Ecology**

- 9.19 The application was accompanied by a Preliminary Ecological Appraisal which advised that there is suitable habitat for bats, breeding birds, hedgehogs, reptiles and other species. Further surveys were also required and have since been undertaken and submitted.
- 9.20 These surveys make a number of recommendations, including the retention and enhancement of the boundary vegetation and buffer zones using native species, a lighting scheme designed to reduce light spill and impact on foraging and commuting bats and the provision of bat and bird boxes and log piles for reptiles.
- 9.21 The proposed works will result in the loss of a small area of foraging/commuting habitat. The habitats present are common and widespread in the area as such impacts are not anticipated to be significant. Conditions will be imposed in relation to the retention of the existing boundary vegetation and buffer zone in addition to the recommended enhancement/compensatory measures.

### **Archaeology**

- 9.22 A consultation response on the previous application (F/YR14/0113/F) requested a programme of investigation be undertaken due to the site being in an area of high archaeological potential.
- 9.23 The application has been accompanied by an archaeological evaluation. The archaeological fieldwork was carried out on the 19th and 20th November 2019 and no archaeological features were revealed on the development site. An email was provided by Cambridgeshire County Council to the author of the report on 13<sup>th</sup> February 2020 which advised that the report was approved and that there were no further archaeological requirements for the site.

### **Land Ownership**

- 9.24 Concerns were raised regarding the potential for the proposed footpath to encroach on third party land. The applicant has undertaken the relevant searches and confirmed that all works can be achieved within Local Highway Authority land.

## **10 CONCLUSIONS**

- 10.1 Policies LP1, LP2 and LP15 of the Fenland Local Plan seek to encourage delivery of a more sustainable transport network. The Fenland Infrastructure Delivery Plan strives to improve rail service usage and proposes a car park near Manea station to support this aim. Subject to the retention and enhancement of the boundary vegetation the proposal is not considered to have a significant detrimental impact on the character of the area. There are no further requirements in relation to

archaeology and it is considered that suitable drainage, landscaping, security, lighting and ecology mitigation can be achieved. As such a favourable recommendation is forthcoming.

## 11 RECOMMENDATION

**Grant** subject to the following conditions;

### Conditions

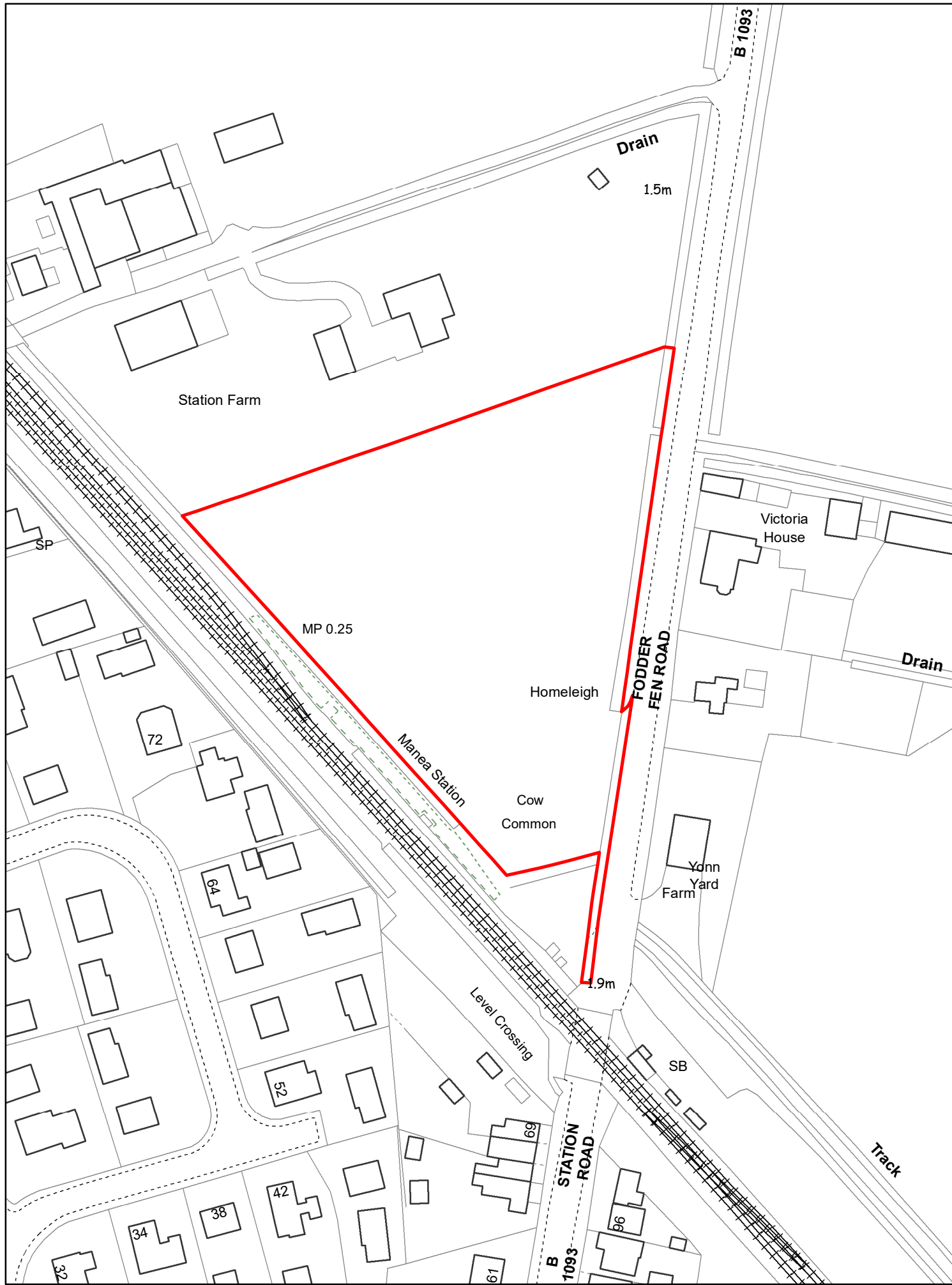
There are four pre-commencement conditions (2, 6, 9 and 13), agreement has been obtained from the applicant to impose these.

1	<p>The development permitted shall be begun before the expiration of 3 years from the date of this permission.</p> <p>Reason - To ensure compliance with Section 51 of the Planning and Compulsory Purchase Act 2004.</p>
2	<p>Prior to the commencement of the development hereby permitted, a surface water drainage scheme for the site, based on sustainable drainage principles, shall be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in full accordance with the approved details prior to the first use of the development hereby approved.</p> <p>The scheme shall be based upon the principles within the agreed Flood Risk Assessment and Drainage Strategy prepared by Create Consulting Engineers Ltd (ref: TT/CC/P19-1853/01) dated March 2020 and shall also include:</p> <p>a) Full results of the proposed drainage system modelling in the 100%, 3.3% and 1% Annual Exceedance Probability storm events (as well as 1% AEP plus climate change), inclusive of all collection, conveyance, storage, flow control and disposal elements, together with an assessment of system performance. This should include pump failure modelling representing full pump failure and 50% storage; b) Detailed drawings of the entire proposed surface water drainage system, including levels, gradients, dimensions and pipe reference numbers; c) Full details of the proposed attenuation and flow control measures; d) Details of overland flood flow routes in the event of system exceedance, with demonstration that such flows can be appropriately managed on site without increasing flood risk to occupants;</p> <p>The drainage scheme must adhere to the hierarchy of drainage options as outlined in the NPPF PPG</p> <p>Reason - To ensure that the proposed development can be adequately drained and to ensure that there is no increased flood risk on or off site resulting from the proposed development, in accordance with Policy LP14 of the Fenland Local Plan 2014.</p>
3	<p>No development shall take place above slab level until a scheme for the provision of external lighting has been submitted to and approved in writing by the Local Planning Authority. Such a scheme shall be</p>

	<p>accompanied by a light impact assessment and include the lighting specifications, proposed positioning, hours of use as well as an Iso Contour to clearly demonstrate what light overspill levels are to be expected from the site. Furthermore, the recommendations of the Preliminary Ecological Appraisal 8946 V4 shall be incorporated. The approved details shall be implemented prior to the first use of the development hereby permitted and retained thereafter in perpetuity.</p> <p>Reason - In order to ensure that the site meets the crime prevention guidelines in accordance with Policy LP17, the residential amenity of surrounding dwellings is protected in accordance with Policy LP16 and ecology is not adversely impacted in accordance with Policy LP19 of the Fenland Local Plan 2014.</p>
4	<p>No development shall take place above slab level until a scheme for the hard and soft landscaping of the site has been submitted to and approved in writing by the Local Planning Authority. Subsequently, these works shall be carried out as approved. The landscaping details to be submitted shall include:-</p> <ul style="list-style-type: none"> <li>b) means of enclosure (taking into account the recommendation of Preliminary Ecological Appraisal 8946 V4)</li> <li>c) car parking layout</li> <li>d) vehicle and pedestrian access and circulation areas</li> <li>e) hard surfacing, other hard landscape features and materials</li> <li>f) existing trees, hedges or other soft features to be retained, including measures to be taken to protect them during construction.</li> <li>g) planting plans, including specifications of species, sizes, planting centres number and percentage mix (taking into account the recommendation of Preliminary Ecological Appraisal 8946 V4)</li> <li>h) details of planting or features to be provided to enhance the value of the development for biodiversity and wildlife</li> <li>i) details of siting and timing of all construction activities to avoid harm to all nature conservation features</li> </ul> <p>Should any plants or trees die, are removed or become seriously damaged or diseased, these shall be replaced in the next planting season with others of similar size and species,</p> <p>The approved hard landscaping scheme shall be carried prior to the first use of the development hereby permitted and the soft landscaping shall be carried out within the first available planting season following completion of the development or alternatively in accordance with a timetable for landscape implementation which has been approved as part of the submitted landscape scheme.</p> <p>Reason: The landscaping of this site is required in order to protect and enhance the existing visual character of the area and to reduce the</p>

	<p>visual, environmental and ecological impacts of the development hereby permitted in accordance with Policies LP16 and LP19 of the Fenland Local Plan 2014.</p>
5	<p>No development shall take place above slab level until a management, maintenance and security plan has been submitted to and approved by the Local Planning Authority in writing. The development shall be carried out as approved in accordance with the specified schedule contained therein.</p> <p>The plan shall include the following details:</p> <ul style="list-style-type: none"> <li>- long term design objectives</li> <li>- management responsibilities</li> <li>- maintenance schedules</li> <li>- security measures, including CCTV</li> <li>- servicing arrangements, including refuse</li> </ul> <p>Reason: To ensure that the site meets the crime prevention guidelines in accordance with Policy LP17 ensure that the development is adequately maintained, managed and serviced in accordance with Policy LP16 of the Fenland Plan 2014.</p>
6	<p>Prior to the commencement of the development hereby permitted, a detailed scheme in relation to a 2m (maximum) wide footway along Fodder Fen Road shall be submitted to and approved in writing by the Local Planning Authority. Such a scheme will be informed and accompanied by an appropriate arboricultural survey. The footpath shall then be constructed in accordance with the approved details prior to the first use of the development hereby permitted, and thereafter retained in perpetuity.</p> <p>Reason: In the interest of satisfactory pedestrian access and to ensure that the surrounding trees are not detrimentally impacted, in accordance with Policy LP15, LP16 and LP19 of the Fenland Local Plan 2014.</p>
7	<p>Prior to the first use of the development hereby permitted, the vehicular access shall be constructed in accordance with submitted plan TP54-11 Rev E</p> <p>Reason: In the interest of satisfactory vehicular access, in accordance with Policy LP15 of the Fenland Local Plan 2014.</p>
8	<p>Within 6-months of the commencement of the development hereby approved full details of a scheme for cycle storage shall be submitted to and approved in writing with the Local Planning Authority. The approved details shall be implemented in full prior to the first use of the development hereby permitted and thereafter retained in perpetuity.</p> <p>Reason - In the interests of security, the convenience of cyclists at the site, and to encourage sustainable forms of transport in accordance with Policies LP15 and LP16 of the Fenland Local Plan 2014.</p>

9	<p>Prior to the commencement of the development hereby approved, a pre-construction mammal walkover shall be undertaken. A report of the findings, along with any necessary mitigation and/or enhancement measures shall be submitted to and approved in writing by the Local Planning Authority. The agreed details shall then be implemented prior to the commencement of works on site and retained thereafter.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
10	<p>The development hereby permitted shall be undertaken in full accordance with the recommendations (Section 4) of the Preliminary Ecological Appraisal 8946 V4.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
11	<p>Prior to the commencement of above ground works in relation to the development hereby permitted, details of a range of bird nest boxes, bat boxes/tiles and log piles shall be submitted to and approved in writing by the Local Planning Authority. Such details shall include the number of boxes, designs and location within the site. The agreed details shall then be implemented prior to first use of the development hereby permitted and retained in perpetuity.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
12	<p>If development has not commenced before May 2022 a repeat reptile survey shall be carried out to confirm whether reptiles are still likely to be absent from the site. Such a survey along with any necessary mitigation or enhancement measures shall be submitted to and approved in writing by the Local Planning Authority. The agreed details shall then be implemented prior to first use of the development hereby permitted and retained in perpetuity.</p> <p>Reason - To protect the habitats of protected species in accordance with Policy LP19 of the Local Plan.</p>
13	<p>Prior to the commencement of the development hereby permitted adequate temporary facilities (details of which shall have previously been submitted to and agreed in writing with the Local Planning Authority) shall be provided clear of the public highway for the parking, turning, loading and unloading of all vehicles visiting the site during the period of construction.</p> <p>Reason: To minimise interference with the free flow and safety of traffic on the adjoining public highway in accordance with Policy LP15 of the Fenland Local Plan 2014.</p>
14	Approved Plans



Created on: 19/08/2020

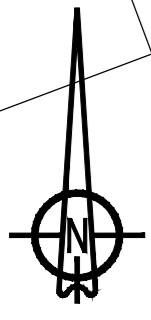
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**F/YR20/0427/F**

Scale = 1:1,250







Station Farm

ALL PARKING BAYS SHOWN ARE 2.5m WIDE X 5.0m LONG

OVERHEAD CABLES AND POLES

Approximate Extent of Overgrown Land

Approximate Extent of Existing Hedge/line Growth

30.0m Buffer Zone from Overgrown Area

30.0m Buffer Zone from Overgrown Area

6.0m Buffer Zone from Hedge/line

6.0m Buffer Zone from Hedge/line

TOTAL PARKING 112 SPACES

OVERHEAD CABLES AND POLES

Approximate Extent of Existing Hedge/line Growth

FODDER FEN ROAD

Homeleigh

Highway Access Crossover (See Note 3)

Private Access Road (See Note 4)

Manea Station

Yonn Yard Farm

- NOTES**
- All levels, chainages and dimensions are in metres, unless otherwise stated.
  - This proposed car parking layout drawing has been created based upon the amalgamation of the following:-
    - A general base plan showing the land identified for car parking and its surrounding area, as taken from Ordnance Survey.
    - An approximate indication of existing features as identified from Google maps.
    - A topographical survey undertaken by Mike Worby Survey Consultancy in September 2015 of Manea Railway Station, its adjoining land and the adjacent Highway approach.
  - Highway access crossover to be constructed to CCC Highways Construction Specification.
  - Private access road to be sealed and drained away from the public highway.

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Revisions					Revisions				
Rev	By	Date	Aprvd	Description	Rev	By	Date	Aprvd	Description
					C	PR	May20		Red line boundary & new link footway adjacent to Fodder Fen Road added. Drawing title amended & "Do Not Scale" removed from Notes for Planning purposes.
E	PR	Jul20		Green shading removed from separate triangular field in SE corner and Red line boundary now shown around overall application site, to suit Planning requirements.	B	PR	Apr20		Disclaimer removed from Note 3 for Planning purposes.
					A	PR	Jan20		Available land area reduced and notional parking layout revised accordingly. Notes & hatching added.
D	PR	Jun20		Highway access crossover & private access road shown, together with new related Notes 3 & 4 to suit CCC/ Planning requirement.	Designed				Eng. Chk PDR JUL '19
					Drawn	PDR	JUL '19		Approved WD JUL '19

Project  
**MANEA RAILWAY STATION**

Title  
**CAR PARKING LAYOUT ON LAND ADJACENT TO THE STATION**



Fenland District Council  
Fenland District Council, Fenland Hall, County Road, March, Cambridgeshire, PE15 8NQ  
Tel: 01354 654321 www.fenland.gov.uk

Scale **1:250 @A1** Drawing No **TP54-11** Rev. **E**